

GENERAL NOTES

1.

ALL SERVICES TO BE INSTALLED AS PER CITY OF CAYUGA CONSTRUCTION AND MATERIAL SPECIFICATIONS MANUAL (LATEST EDITION) AND MINISTRY OF THE ENVIRONMENT GUIDELINES (LATEST EDITION).

2.

APPROVAL OF THIS DRAWING IS FOR MATERIAL ACCEPTABILITY AND COMPLIANCE WITH MUNICIPAL AND PROVINCIAL SPECIFICATIONS AND STANDARDS ONLY. APPROVAL AND INSPECTION BY THE CITY OF THE WORKS DOES NOT CERTIFY THE LINE AND GRADE OF THE WORKS, AND IT IS THE OWNER'S RESPONSIBILITY TO HAVE THEIR ENGINEER CERTIFY THIS ACCORDINGLY.

ROAD WORK

1

FINAL ROADWAYS

a)

CROSS-FALL TO BE 2.0%.

b)

STANDARD DEEP STRENGTH PAVEMENT (40mm HM 3, 80mm HL 8 ON 150mm GRANULAR 'A' & 300mm GRANULAR 'B', TYPE II 100% CRUSHED AGGREGATE) FOR TYPICAL URBAN RESIDENTIAL STREETS.

c)

FOR MAJOR COLLECTOR ROADS (NAME OF ROAD'S) TYPICAL PAVEMENT PROFILE SHALL BE 40mm HM 3(HD), 100mm HL 8(HS) ON 150mm GRANULAR 'A' & 300mm GRANULAR 'B', TYPE II 100% CRUSHED AGGREGATE.

d)

MANHOLES AND CATCH BASINS SHALL BE INSTALLED FLUSH WITH THE BASE COURSE ASPHALT (HL 8).

e)

MANHOLES TO BE ADJUSTED TO MATCH FINAL LIFT OF ASPHALT.

f)

FOR MANHOLE AND CATCH BASIN TOP ADJUSTMENTS, ALL PERMANENT ADJUSTMENTS ARE TO BE POURED IN PLACE.

g)

FINAL ASPHALT COURSE (HM 3) SHALL BE PLACED A MIN. OF ONE YEAR AFTER THE INSTALLATION OF THE ASPHALT BINDER COURSE.

LOT GRADING NOTES

GENERAL GRADING NOTES:

1.

THE WRITTEN PERMISSION REQUIRED FROM THE ADJACENT LANDOWNER SHALL BE OBTAINED PRIOR TO ENTERING THE LANDS. SHOULD PERMISSION NOT BE OBTAINED OR IS WITHDRAWN PRIOR TO COMMENCING THE WORK, THEN THE DEVELOPER SHALL LIMIT HIS ACTIVITIES TO THE LIMITS OF THE DEVELOPMENT SITE.

COMPACTION REQUIREMENTS

a)

ALL BEDDING AND BACKFILL MATERIAL, ROAD SUB-GRADES AND GENERALLY ALL MATERIAL USED FOR LOT GRADING AND FILL SECTIONS, ETC., SHALL BE COMPACTED TO MIN. 95% SPD (UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER). ALL MATERIAL SHALL BE PLACED IN LAYERS NOT EXCEEDING 300mm LIFTS.

b)

ALL GRANULAR ROAD BASE MATERIALS SHALL BE COMPACTED TO 95% SPD.

c)

FOR ALL SEWERS AND WATERMAINS IN FILL SECTIONS, THE COMPACTION SHALL BE CERTIFIED BY A GEOTECHNICAL ENGINEER PRIOR TO LAYING OF PIPE.

SILTATION AND EROSION CONTROL

A.

ALL SILTATION CONTROL MEASURES SHALL BE CLEANED AND MAINTAINED AFTER EACH RAINFALL AND ALSO WEEKLY AS DIRECTED AND TO THE SATISFACTION OF THE CITY OF CAYUGA.

B.

ADDITIONAL SILT CONTROL LOCATIONS MAY BE REQUIRED AS DETERMINED BY THE CITY OF CAYUGA.

C.

PROTECT ALL DISTURBED AND EXPOSED AREAS AS A RESULT OF CONSTRUCTION. STORM WATER MEASURES DURING CONSTRUCTION TO BE UTILIZED TO ENSURE SUITABLE DRAINAGE WHILE MINIMIZING EROSION. STOCKPILES ARE TO BE SEEDED OR COVERED WITH VEGETATIVE GROWTH FOR THE DURATION OF CONSTRUCTION.

D.

PROTECT ALL MANHOLES, AND PIPE ENDS (EXISTING AND NEW) FROM SEDIMENT INTRUSION WITH GEOTEXTILE CLOTH (TERRAFIX 270). ALL CATCHBASINS TO HAVE SILTSACK AS PER THE ATTACHED DETAILS.

E.

PREVENT WIND-BLOWN DUST TO THE BEST OF THE CONTRACTORS ABILITY. KEEP SOIL DAMP DURING DRY WHETHER OR BY OTHER MEANS NECESSARY TO COMPLETE THE WORK.

F.

EROSION CONTROL STRUCTURES TO BE MONITORED REGULARLY BY CONTRACTOR AND ANY DAMAGE REPAIRED IMMEDIATELY. SEDIMENTS TO BE REMOVED WHEN ACCUMULATIONS REACH A MAXIMUM OF ONE THIRD (1/3) THE HEIGHT OF THE SILT FENCE.

G.

ALL EROSION CONTROL STRUCTURES TO REMAIN IN PLACE UNTIL ALL DISTURBED GROUND SURFACES HAVE BEEN RE-STABILIZED EITHER BY PAVING OR RESTORATION OF VEGETATIVE GROUND COVER.

H.

THE CONTRACTOR IS RESPONSIBLE FOR REMOVING SEDIMENTS FROM THE MUNICIPAL ROADWAY AND SIDEWALKS AT THE END OF EACH WORK DAY.

SITE GRADING

1.

NATIVE BACKFILL MATERIAL SHALL BE COMPACTED TO 98% STANDARD PROCTOR DENSITY. GRANULAR BACKFILL MATERIAL SHALL BE PLACED IN LAYERS 150mm IN DEPTH AND COMPACTED TO 98% STANDARD PROCTOR DENSITY.

2.

REFER TO SITE PLAN FOR LAYOUT DIMENSIONS AND DETAILS.

3.

PAVEMENT SHALL BE:

PAVEMENT COMPONENT	THICKNESS (mm)
HEAVY DUTY ASPHALT (ROADWAY & DRIVE ISLES)	
ASPHALT SURFACING -HL3	40
ASPHALT SURFACING -HL8	50
GRANULAR "A" BASE	150
GRANULAR "B" SUBBASE	350

4.

SUBMIT ASPHALT MIX DESIGN AND TRIAL MIX TEST RESULTS TO CONSULTANT FOR APPROVAL.

5.

PROOF ROLLING OF SUBGRADE SHALL BE INSPECTED BY THE GEOTECHNICAL CONSULTANT.

6.

PLACE GRANULAR BASE TO COMPACTED THICKNESS AS INDICATED. DO NOT PLACE FROZEN MATERIAL.

7.

ASPHALT MATERIALS SHALL BE ROLLED AND COMPACTED TO A MINIMUM OF 97% MRD.

8.

PROOF ROLLING OF ASPHALT SHALL BE INSPECTED BY THE GEOTECHNICAL CONSULTANT.

9.

NO PAVING WILL BE ALLOWED DURING RAIN OR WET SUBGRADE AFTER RAIN.

SITE SPECIFIC NOTES

1.

ALL PATCHING TO MEET OPSS PROV 307 FOR PATCHING OF ASPHALT PAVEMENT.

2.

ALL HOT PATCHING SHALL MEET THE REQUIREMENTS OF THE OPSS MUNI 1150.

3.

COLD OR HOT PATCH OR BOTH CAN APPLY DEPENDING ON THE WEATHER CONDITIONS. IN CASE COLD PATCH IS APPLIED AND THEN THE HOT PATCH IS APPLIED, THE AREA AROUND THE COLD PATCH IS TO BE SAW CUT AND TO BE CLEANED AND COMPACTED PRIOR TO THE APPLICATION OF THE HOT PATCH.

4.

FOR AREAS MORE THAN 4 m2 MECHANICAL COMPACTION EQUIPMENT TO BE USED.

SIDE VIEW INSTALLED

EXPANSION RESTRAINT

INSTALLATION DETAIL

1" REBAR FOR BAG REMOVAL FROM INLET (REBAR NOT INCLUDED)

OPTIONAL OVERFLOW

SILTSACK @ DUMP LOOPS (REBAR NOT INCLUDED)

GRATE

CURB OPENING

FOAM

DEPTH = D

LENGTH = L

WIDTH = W

TYP SILT TRAP DETAIL

SCALE: N.T.S.

SAW CUT THE EXISTING ASPHALT AND THE COLD PATCH (IF ANY) AND PROVIDE HOT PATCH

LAP JOINT

LAP JOINT

PROVIDE MACHINE BASED COMPACTION TO MEET THE COMPACTION REQUIREMENTS

TYP HOT PATCH DETAIL

SCALE: N.T.S.

EXISTING ASPHALT

PROPOSED ASPHALT AS PER CITY STANDARDS

750mm MIN.

EXISTING ASPHALT TO BE MILLED 40mm AND RESURFACE WITH 40mm TOP ASPHALT

EX. GRANULAR BASE

GRANULAR "A"

GRANULAR "B" MIN.

EX. SUBGRADE

BASE COURSE ASPHALT

TOP ASPHALT

LIMIT OF ASPHALT (ASPHALT JOINT)

NOTE: PROPOSED PAVEMENT DESIGN IN THE ROAD SHALL MATCH EXISTING DEPTHS AND CONFORM TO CITY STANDARDS

TYP ASPHALT LAP JOINT DETAIL

SCALE: N.T.S.

MANTECON PARTNERS

STRUCTURAL MECHANICAL ELECTRICAL CIVIL ENGINEERS

15 Foundry Street, Dundas, ON, L9H 2V6

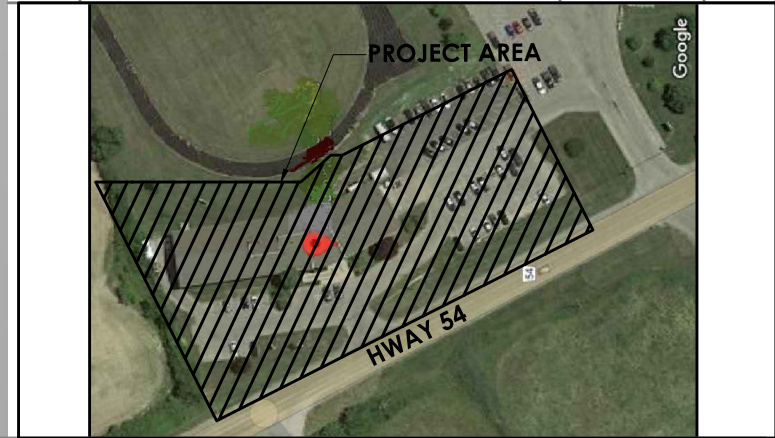
Phone: (905)648-0373

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REVIEW ALL DRAWINGS AND VERIFY ALL DIMENSIONS AT THE SITE. DO NOT SCALE THE DRAWINGS. REPORT ALL DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING WITH ANY CONSTRUCTION OR SHOP FABRICATION. ALL DRAWINGS, SPECIFICATIONS AND RELATED DOCUMENTS ARE THE COPYRIGHT PROPERTY OF "MANTECON PARTNERS" AND MUST BE RETURNED UPON REQUEST. REPRODUCTION OF DRAWINGS, SPECIFICATIONS AND RELATED DOCUMENTS IN PART OR WHOLE IS FORBIDDEN WITHOUT THE ENGINEER'S WRITTEN PERMISSION.

3.	ISSUED FOR TENDER	2025-03-31	Y.T.
2.	ISSUED FOR EMERGENCY REPAIR TENDER	2025-02-07	Y.T.
1.	ISSUED FOR EMERGENCY REPAIR REVIEW	2025-01-15	Y.T.

NO.	ISSUED	DATE	BY
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CLIENT

Colliers INTERNATIONAL

Colliers Project Leaders

Infrastructure Ontario

PROJECT 1101603-274031

ONTARIO PROVINCIAL POLICE

CAYUGA DETACHMENT  
72 HALDIMAND COUNTY HWY 54, CAYUGA, ON, N0A 1E0  
EMERGENCY HOT PATCH

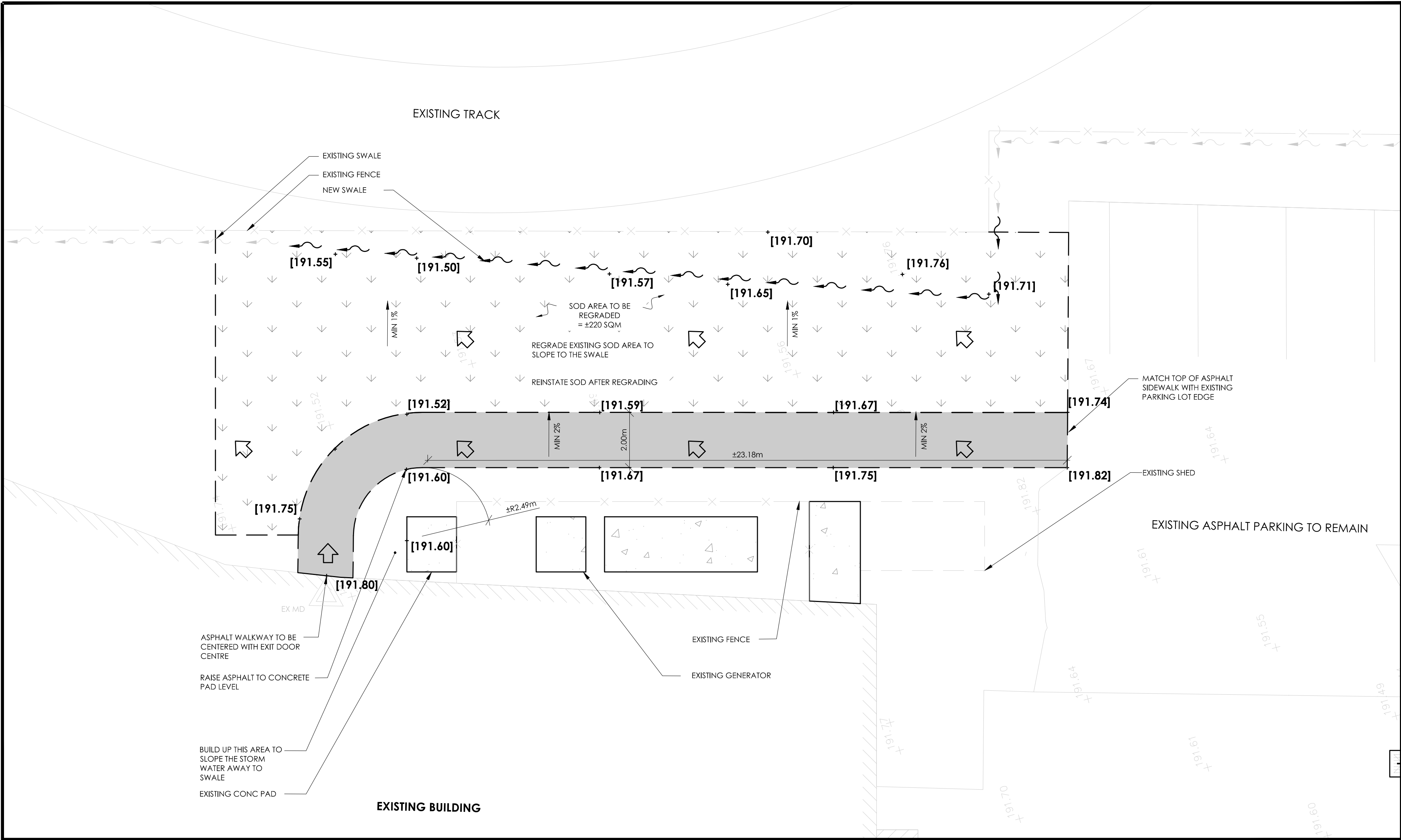
GENERAL NOTES

DRAWN BY: H.S.	SCALE: N.T.S.
CHECKED BY: Y.T.	DRAWING NUMBER:
DATE: 2024-02-23	C0.0
PROJECT NUMBER: 23-137	









**SIDEWALK REPAIR**  
SCALE: 1:100

LEGEND - SITE GRADING	
THIS LEGEND OF SYMBOLS REPRESENTS MANTECON PARTNERS INC. STANDARD/GENERIC LEGEND. ALL SYMBOLS MAY NOT APPEAR ON DRAWINGS.	
REFER	DESCRIPTION
	PROPERTY LINE
	EXISTING BUILDING
	EXISTING ASPHALT TO BE REMOVED AND RESURFACED
	EXISTING CONCRETE
	EXISTING SOD
	EXISTING ELEVATION
	PROPOSED ELEVATION
METRIC: DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048	

STRUCTURAL MECHANICAL ELECTRICAL CIVIL ENGINEERS  
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NO.	ISSUED	DATE	BY

**PROJECT 1101603-274031**

CIENT:  
**ONTARIO PROVINCIAL POLICE**

PROJECT:  
**CAYUGA DETACHMENT  
72 HALDIMAND COUNTY HWY  
54, CAYUGA, ON, N0A 1E0  
EMERGENCY HOT PATCH**

DRAWING TITLE:  
**SIDEWALK REPAIR**

DRAWN BY: <b>H.S.</b>	SCALE: <b>AS NOTED</b>
CHECKED BY: <b>Y.T.</b>	DRAWING NUMBER:
DATE: <b>2024-02-23</b>	<b>C2.0</b>
PROJECT NUMBER: <b>23-137</b>	